

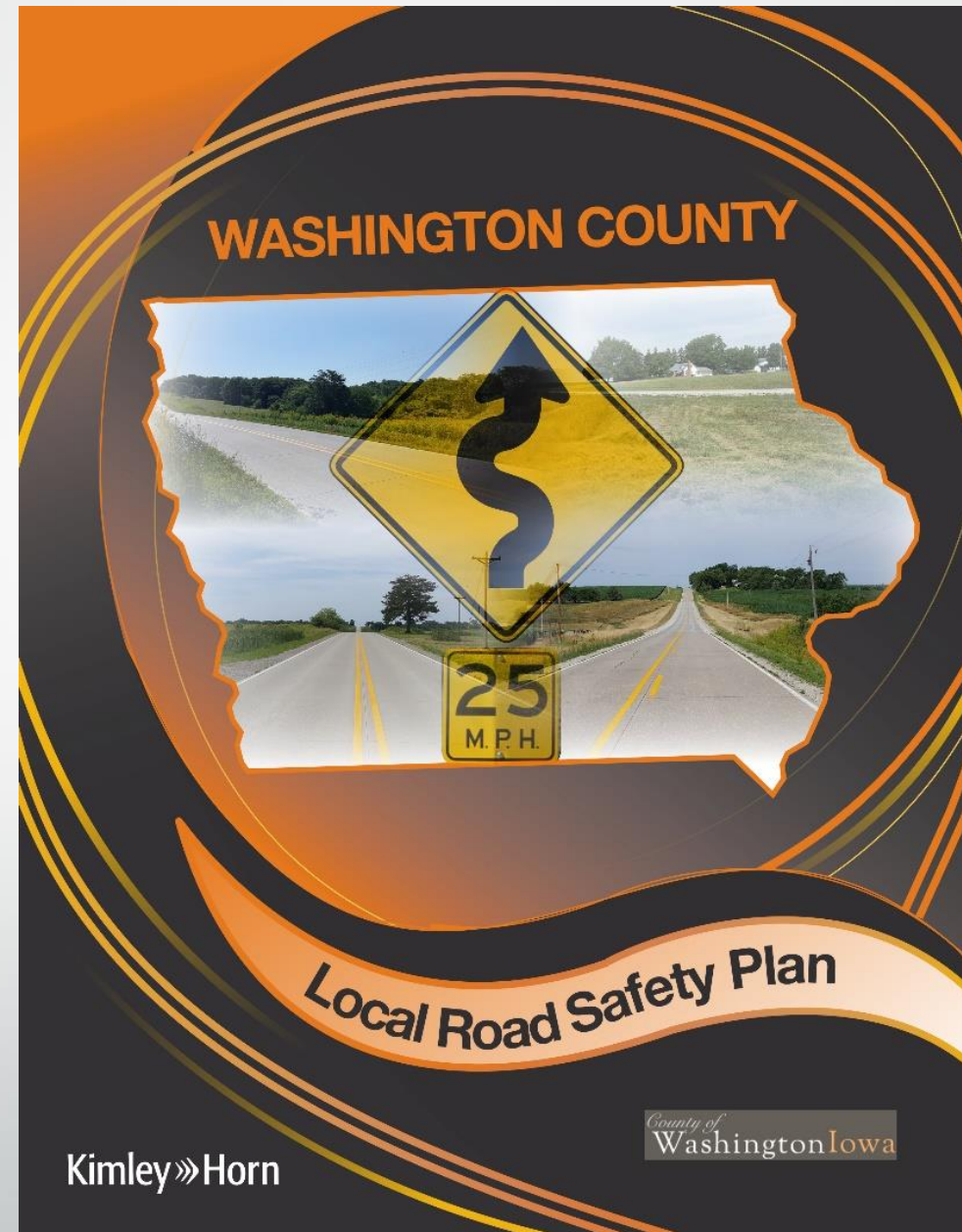
How Can Local Agencies Help Address Roadway Fatalities?

Efforts In Iowa

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Iowa Efforts

- Local Road Safety Plans
- Funding Opportunities
- Washington County Specific



What is an LRSP and its Purpose?

- A document that analyzes the roads system and provides a basis for installing proactive safety improvements
- Defines a locally focused plan to help local officials make informed & prioritized safety decisions
 - Produces a prioritized list of safety improvement projects
 - Focused on proactive improvements versus reactionary analysis
 - Beyond chasing the “black spots”

Benefits of a LRSP

- Coordination between various agencies within the county
- Use of the results of the analysis to leverage and apply for funding
- Focus on all the five E's of safety
 - Engineering,
 - Emergency response,
 - Education,
 - Enforcement, and
 - Everyone



Why did Iowa do LRSPs?

- Over 50% fatal and serious injury crashes occur on the local system
- County roads are less “forgiving”
 - Narrow pavement
 - Steep side slopes
 - Less clear zone area
- Iowa learned about LRSPs at a Peer Exchange in Minnesota in 2013
- Used state and federal safety funds to develop LRSPs
 - No cost to participate
 - Involved in development and review with consultant Kimley Horn & Iowa DOT

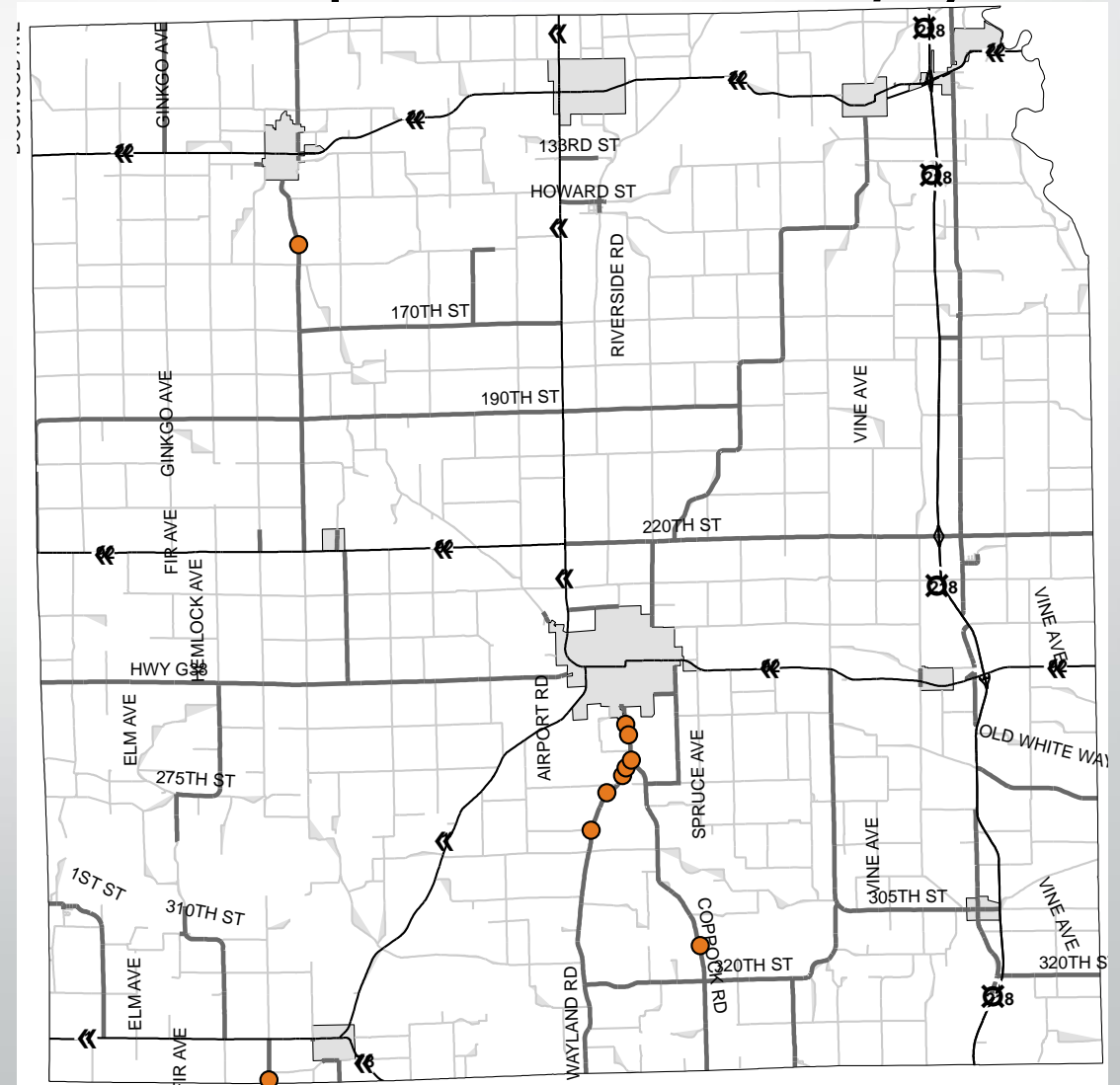


County LRSP Stats

- Each county had a project sheet prepared for 10 segments, 10 intersections and 10 curves
 - Over 1,800 project sheets
 - Analyzed over:
 - 10,500 miles of paved roads
 - 17,100 intersections
 - 6,000 curves
 - At least \$1M in improvements identified for each county

Washington County LRSP completed in 2016/17

- 13 Segments ~\$6,700,000 estimated project costs
- 11 Intersections ~ \$1,350,000 estimated project costs
- 10 Curves ~ \$177,000 estimated project costs



Iowa Safety Funding Opportunities

- Highway Safety Improvement Program – Secondary \$2.0 million/year - Federal
 - Established to fund low-cost, systemic safety improvements on rural roads
 - On Secondary Roads with current crash history or at-risk characteristics
 - Projects cost approximately \$10,000/mile and along corridors
- Traffic Safety Improvement Program \$8.0 million/year - State
 - Provides funding for traffic safety improvements or studies on any public road
 - Site-specific Improvements: projects intended to increase traffic safety at a specific site
 - Traffic Control Device: purchase materials for new installation or replacement of obsolete devices
 - Studies and outreach: transportation safety research, studies or initiatives

Iowa Safety Funding Opportunities cont.

- County-State Traffic Engineering Program
 - Solve traffic operation and safety problems at intersections of county and state roads
 - Spot improvements – limited to a single location, County/State split 45/55
 - Linear improvements – for locations where spot improvement is inadequate, County/State split 70/30 or 40/60
- Traffic Engineering Assistance Program
 - Provides traffic engineering expertise to local governments
 - Identifies cost-effective traffic safety & operational improvements
 - Identifies potential funding sources to implement recommendations
 - No match required, but will need to assist in data collection

Washington County Specific Efforts

- Paved Shoulders – \$2.245 Million in TSIP funds
 - Pave 4 foot wide
 - Install Rumble strips
- Centerline Rumble Strips– \$88,000 in HSIP Funds
- Upgrade Traffic Signs – \$70,000 in Sign Replacement Program Funds
 - Upgrade chevrons & curve warning signs along paved roads
 - Replace signs to meet retroreflectivity requirements



Washington County Specific Efforts

- Overpass with on/off ramp installed at intersection with divided four lane highway
 - Joint project with Iowa DOT
 - Utilized \$500,000 in High Risk Rural Roads funding
- Pavement markings
 - Centerline every other year
 - Half of edge line every other year

Washington County Specific Efforts

- Portable Traffic Lights
 - Reduce need for flaggers in work zones
 - Easier for traveling public to understand
- Older pavements - steep foreslopes
 - Flatten slopes with material from ditching projects
- Newer pavements - 10 foot wide rock shoulders
 - Room for slow moving farm traffic to get off road
 - "Recovery" area for traffic veering off road



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